

SOUTH TRANSCONA SECONDARY PLAN

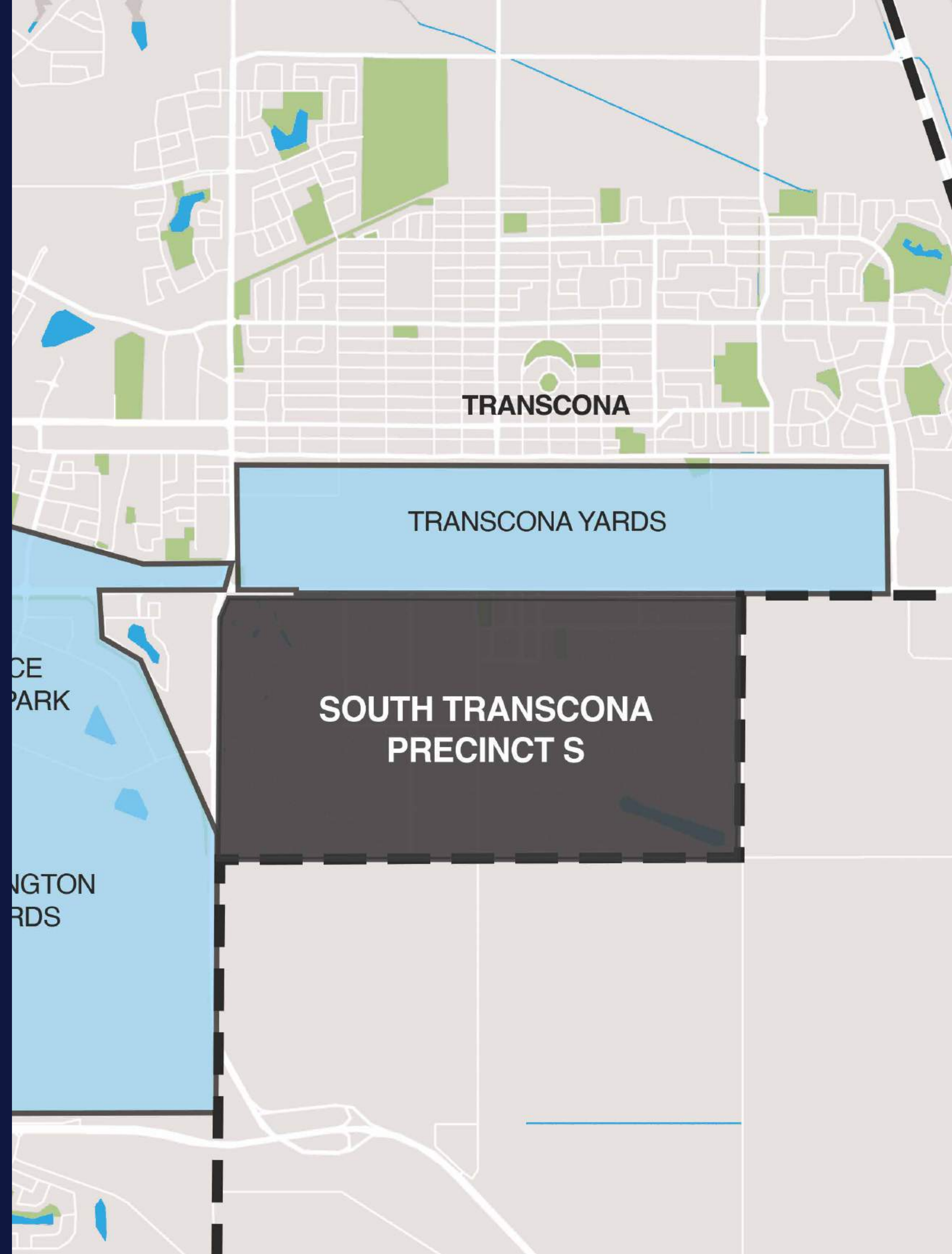
October 2025

Two private developers are front-ending the costs for the process to develop a Secondary Plan for the South Transcona area.



Funding has also been provided by the East Kildonan-Transcona Community Committee and Councillor Russ Wyatt via the Land Dedication Reserve Fund, which can be used for park improvements and Parks & Recreation Planning.

The final round of consultation will be held in November, taking into account feedback received at this event, the outcomes of ongoing studies, and the City of Winnipeg review. The plan will go to City Council for consideration and approval.



SOUTH TRANSCONA PLAN AREA

TRANSCONA YARDS

DUGALD ROAD

PLESSIS ROAD

MURDOCK ROAD

ST. BONIFACE ROAD

RM OF SPRINGFIELD

TRANSCONA GOLF CLUB

BUHLER RECREATION PARK

LAKE SHIRLEY WATER SKI PARK

PLAN AREA = 1313 ACRES

A

The Acreages

Approximately 404 acres characterized by large, estate-style lots.

B

The Hamlet

Approximately 180 acres and currently characterized by serviced residential lots.

C

West Development Area

Approximately 165 acres of major developer-owned land.

D

East Development Area

Approximately 178 acres of major developer-owned land.

VISION

A SUSTAINABLE, CONNECTED COMMUNITY THAT CELEBRATES NATURE AND BELONGING.

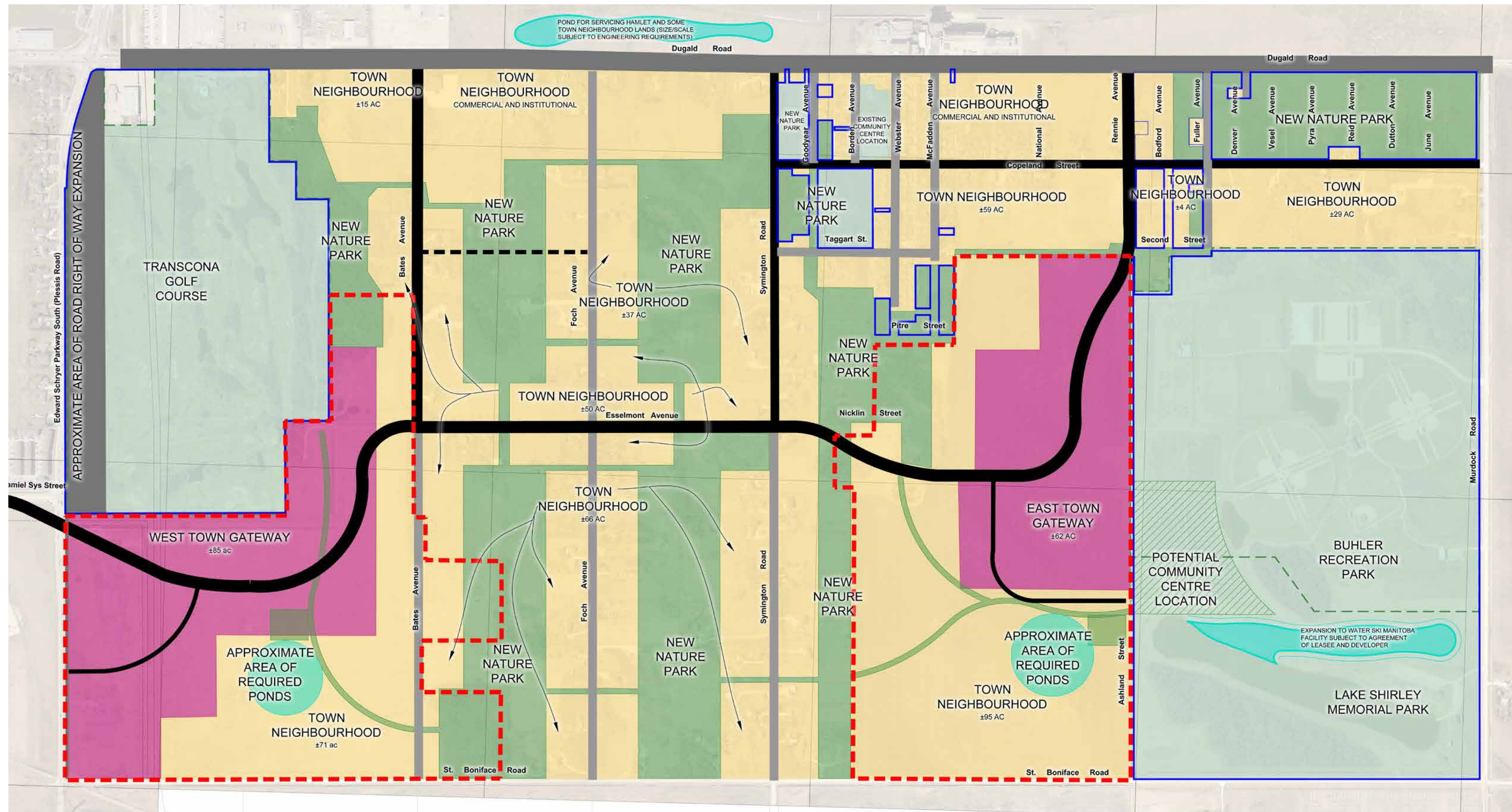
GOAL

South Transcona will be a 21st Century model development, showing how Winnipeg communities can grow and change in ways that work from a development, environmental, transportation, and quality of life perspective, including the following:

- ✓ **PEOPLE & HOMES:**
Home to 20,000 residents and 7,000–10,000 homes on 1,313 acres. A full spectrum of housing types including existing single-family, new single-family, duplexes, townhomes, and apartments. Affordable and market options that let people age in place.
- ✓ **NATURE & ENVIRONMENT:**
A new Nature Park and naturalized drainage system that preserves local ecology. Designed for resilience and biodiversity.
- ✓ **COMMUNITY & CULTURE:**
A new Community Centre and gathering places that foster belonging, wellness, and celebrates local heritage.
- ✓ **MOVEMENT & CONNECTION:**
A complete network for walking, cycling, transit, and vehicles. The timing and connection of new roads, infrastructure, and services will be a high priority and timed with development.
- ✓ **INFRASTRUCTURE & ENERGY:**
Urban-standard services (water, wastewater, stormwater) designed for efficiency and climate adaptation including exploration of geo-thermal district energy.
- ✓ **PHASING & IMPLEMENTATION:**
Clear cost-sharing, demand-based phasing, and innovative funding mechanisms to deliver community assets as development proceeds.



SOUTH TRANSCONA CONCEPTUAL LAND USE PLAN



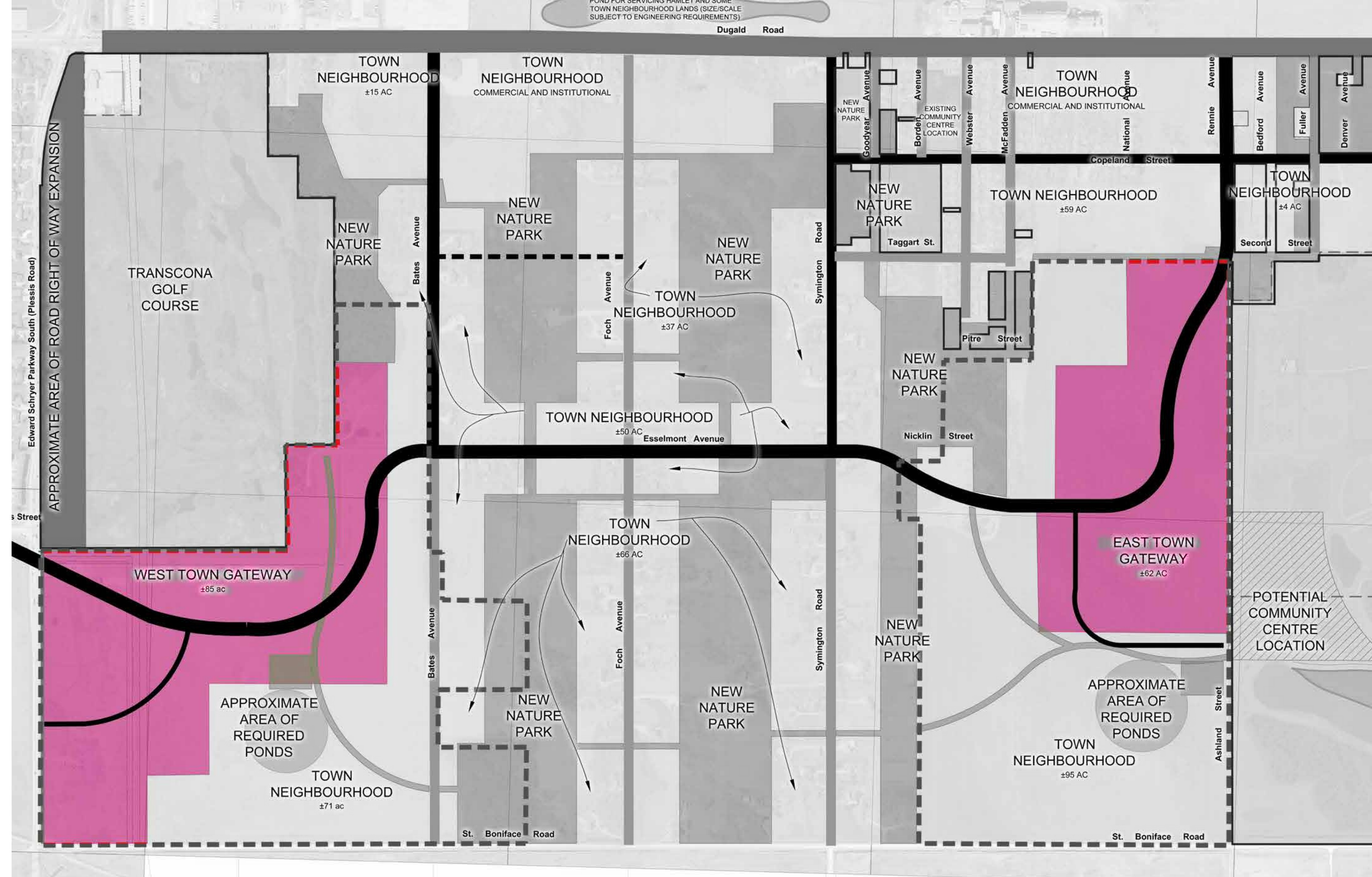
- Densities have been increased in both the Acreages and the Hamlet to allow homeowners to maximize land value, while ensuring that no property owner is required to subdivide or develop their land.
- By allowing low to high densities across the Town Neighbourhood, small property owners avoid the need for an expensive Secondary Plan amendment. while allowing options **if they choose** to develop their land.



LAND USE PLAN

TOWN GATEWAY

A mixed-use area that acts as a “gateway” to the neighbourhood, complete with a mix of commercial, multi-family, high- medium- and low-density dwelling types. Densities will be focused along major collectors and arterials and transition down towards the Town Neighbourhood, with policies put into place to protect existing homeowners.



Mixed-Use

Built-form will step down predictably: taller forms and mixed use within Gateways along major frontages to mid-scale apartments and stacked townhouses along collectors; and ground-oriented housing transitioning to existing single-detached fabric.



Up to +10 Storeys



Grocery Store



Neighbourhood Commercial



Duplex



Triplex



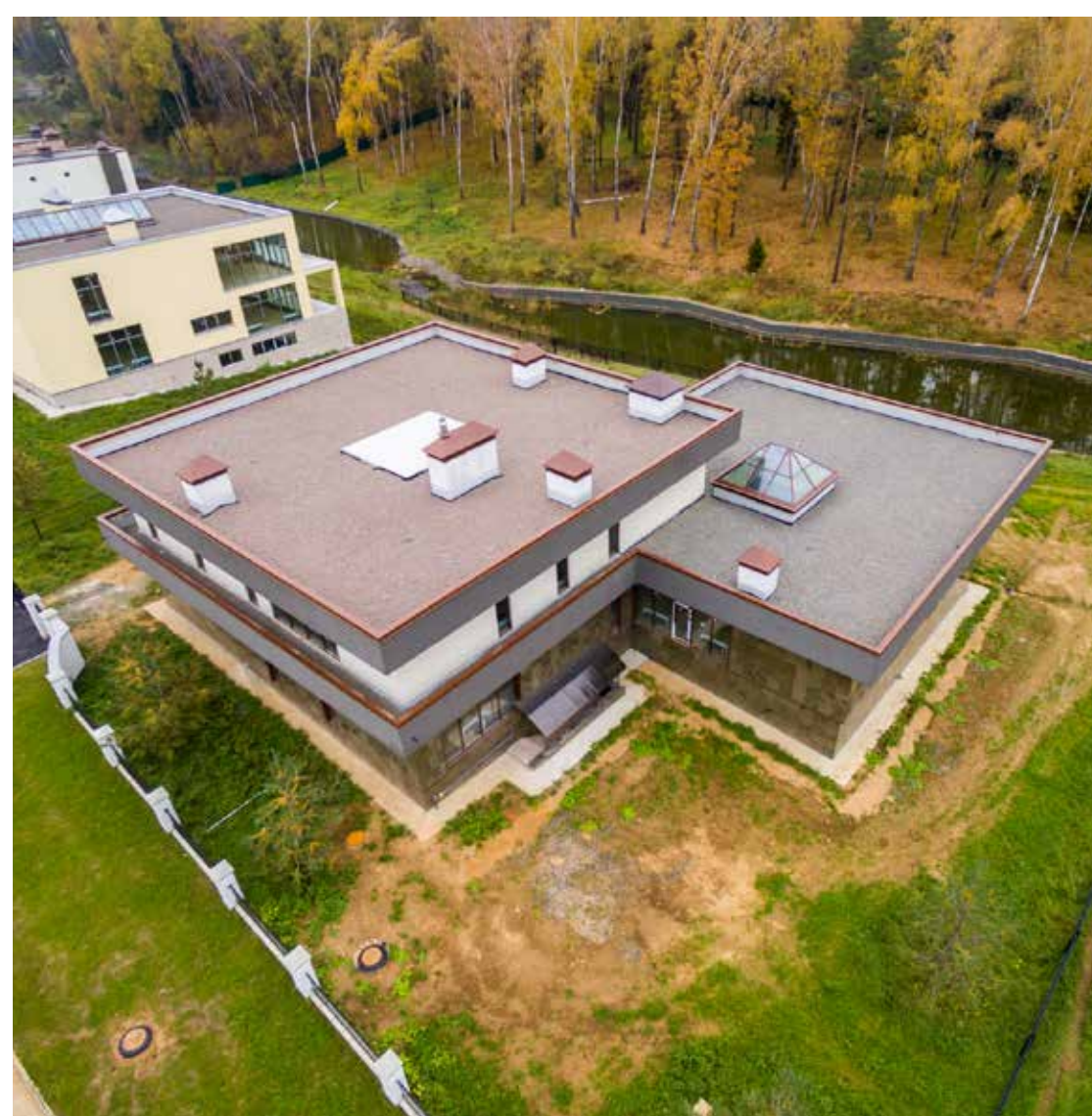
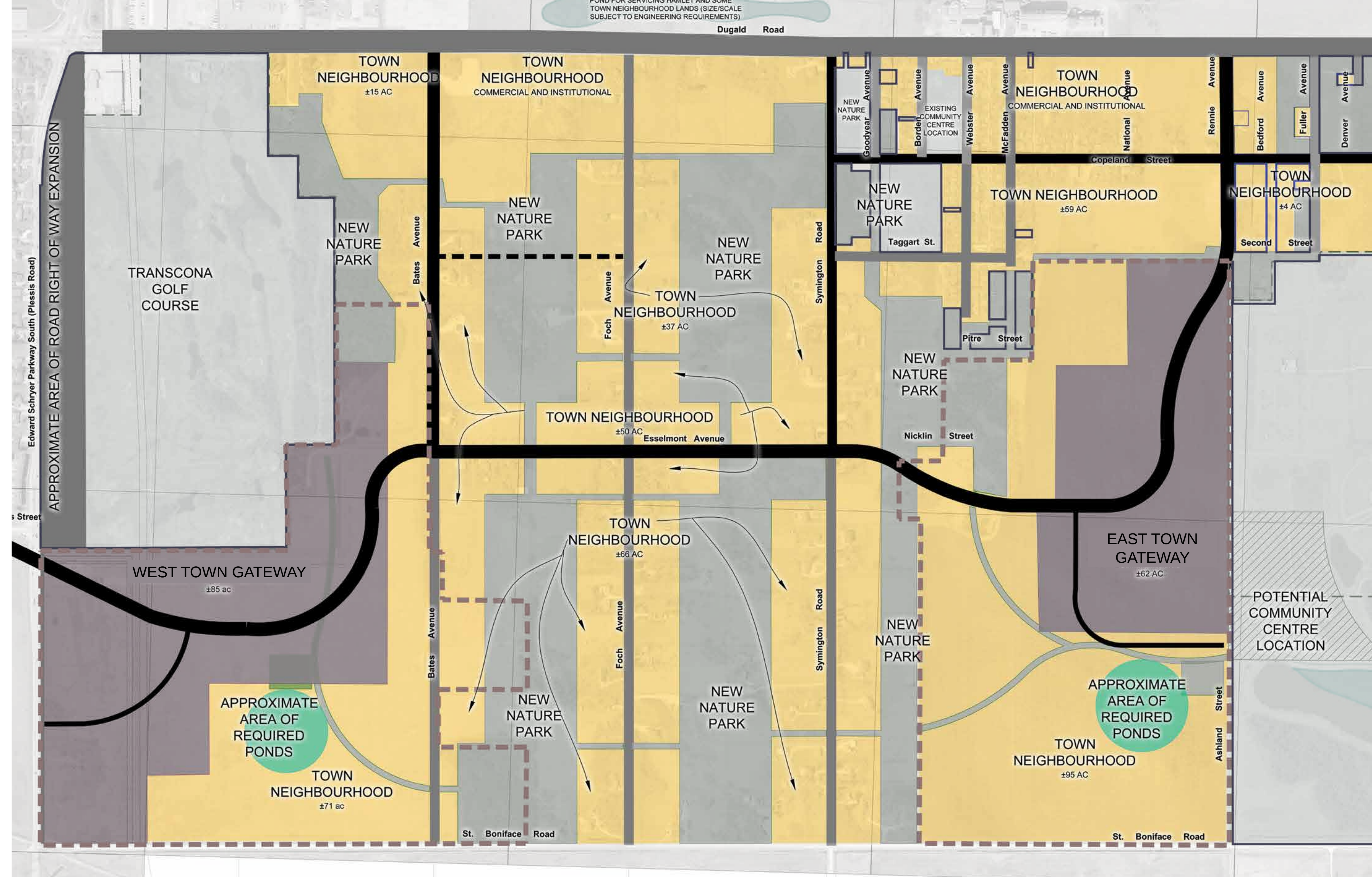
Fourplex

LAND USE PLAN

TOWN NEIGHBOURHOOD

A primarily residential area with a mix of housing types and sizes from large-lot single-family to multi-family complexes, with policies in place to protect existing homeowners.

Some commercial will be focused along major roads (Dugald and Esselmont) with places of worship, education, and other institutions to support the community.



Single Family



Townhouses



Mid-Rise



Duplex & Triplex

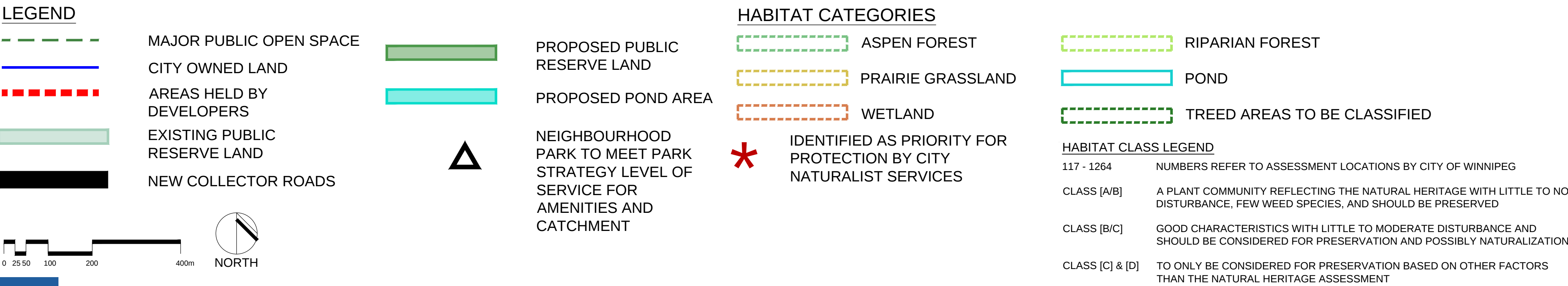
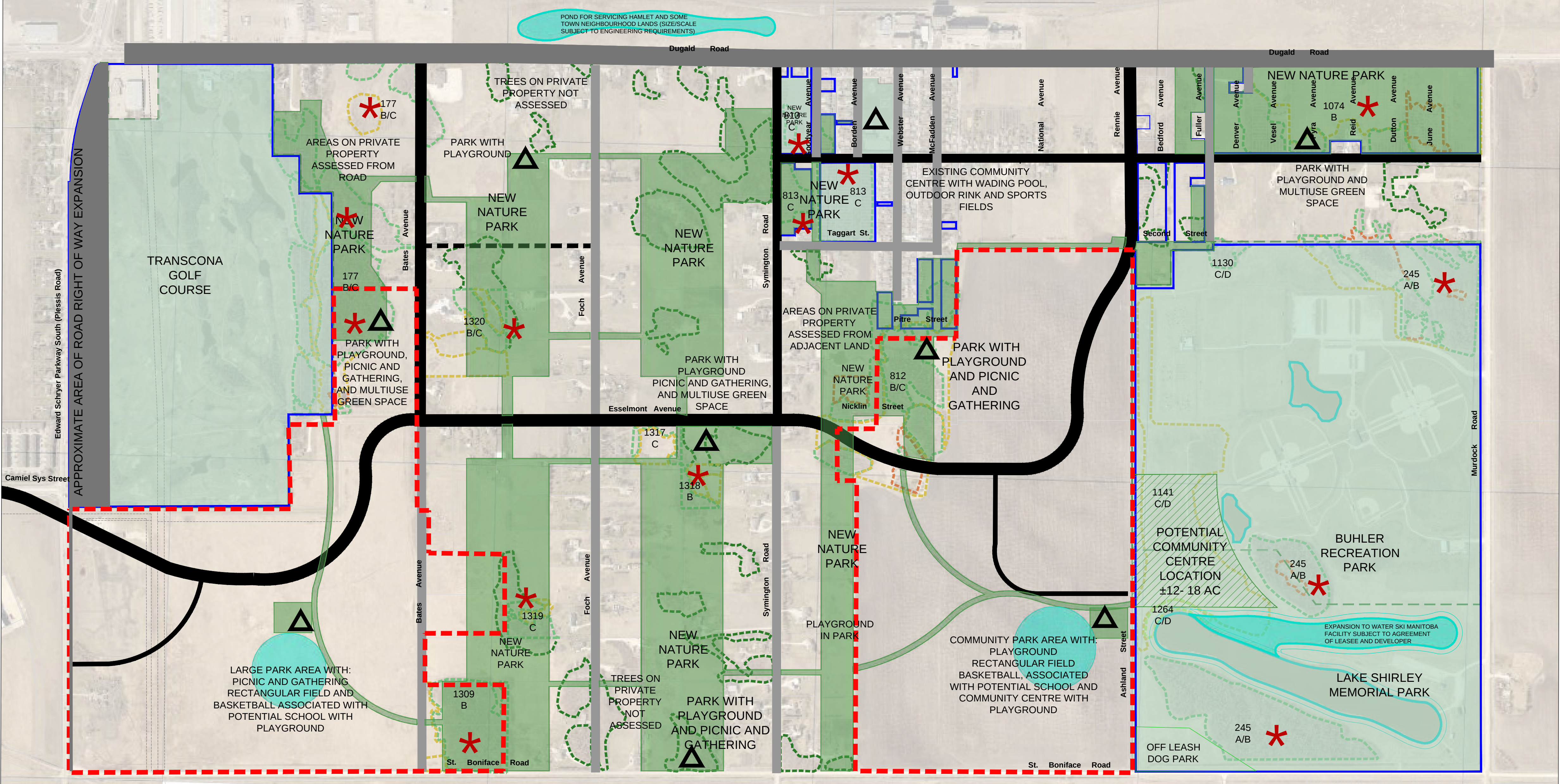


Fourplex

Any transition in existing areas, is expected to occur over time, and will be contingent upon the provision of appropriate services, such as municipal sewer, water, and stormwater systems as well as adequate roadway and transit connections subject to Council approval.

PARKS

PARKS AND NATURAL AREAS PLAN

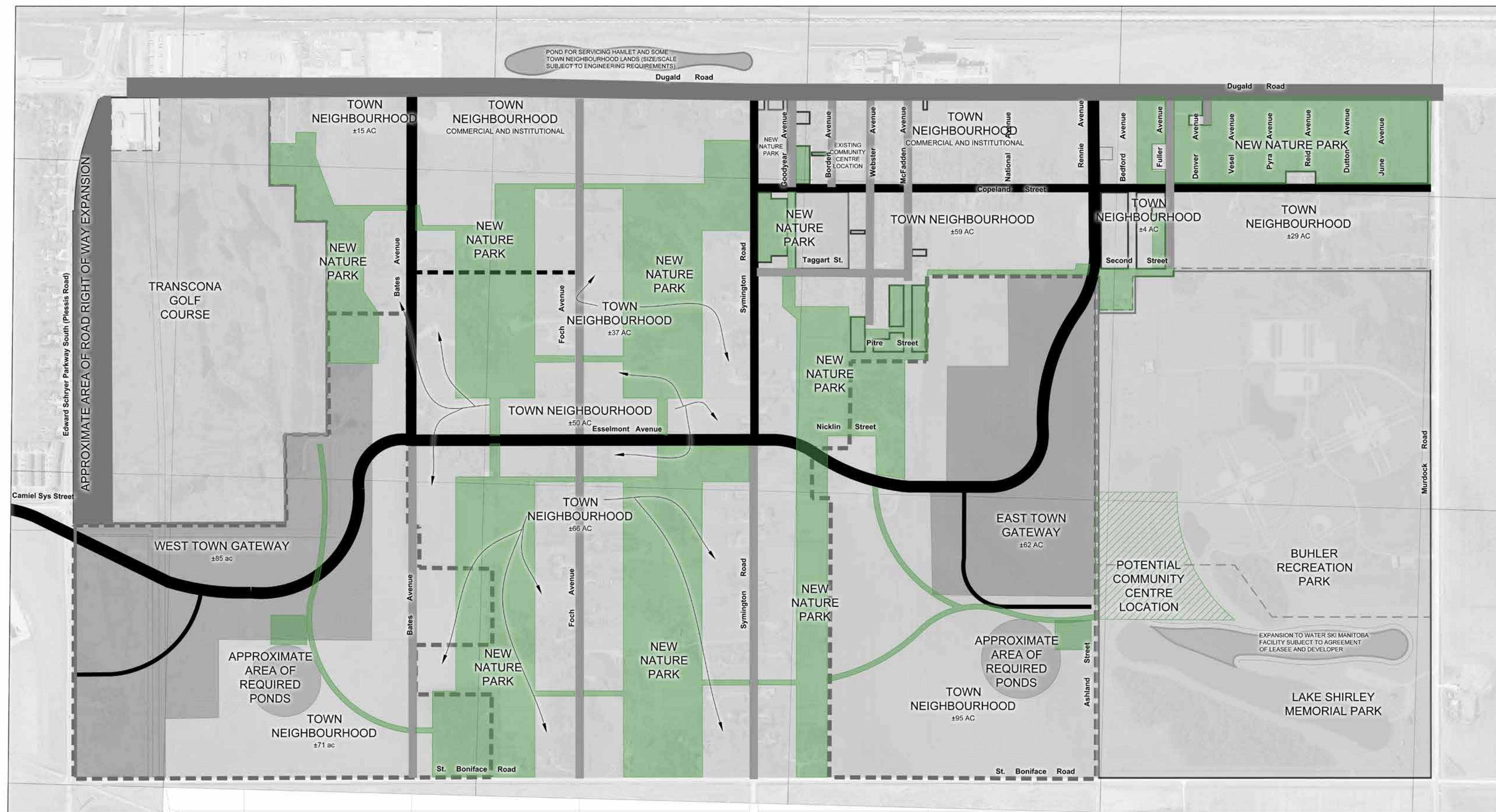


- A new Community Centre site would be co-located with indoor and outdoor recreation amenities.
- Naturalized corridors are long corridors that focus on preserving and celebrating local flora and fauna in low impact ways (ie. trails, signage, places to sit and reflect).

Note: The approximately 200-acre Nature Park will help Winnipeg meet its commitments in the Montreal Pledge for Cities United in Action for Biodiversity which it signed in 2023. The City has set a target of adding 1,000 acres of new greenspace by 2045.

PARKS

NATURE PARK



- Over time, a linear Nature Park system will emerge to protect flora and fauna and form continuous natural corridors.
- It can be incrementally created through land dedications, development agreement obligations via subdivisions, targeted City capital, and third-party grants.
- This vision will be achieved through trading off land for park in lieu of increasing density.

Note: The approximately 200-acre Nature Park will help Winnipeg meet its commitments in the Montreal Pledge for Cities United in Action for Biodiversity which it signed in 2023. The City has set a target of adding 1,000 acres of new greenspace by 2045.

PARKS

PARKS AREAS CONCEPT PLAN

SOUTH TRANSCONA SECONDARY PLAN DRAFT PARK AND POND AREAS

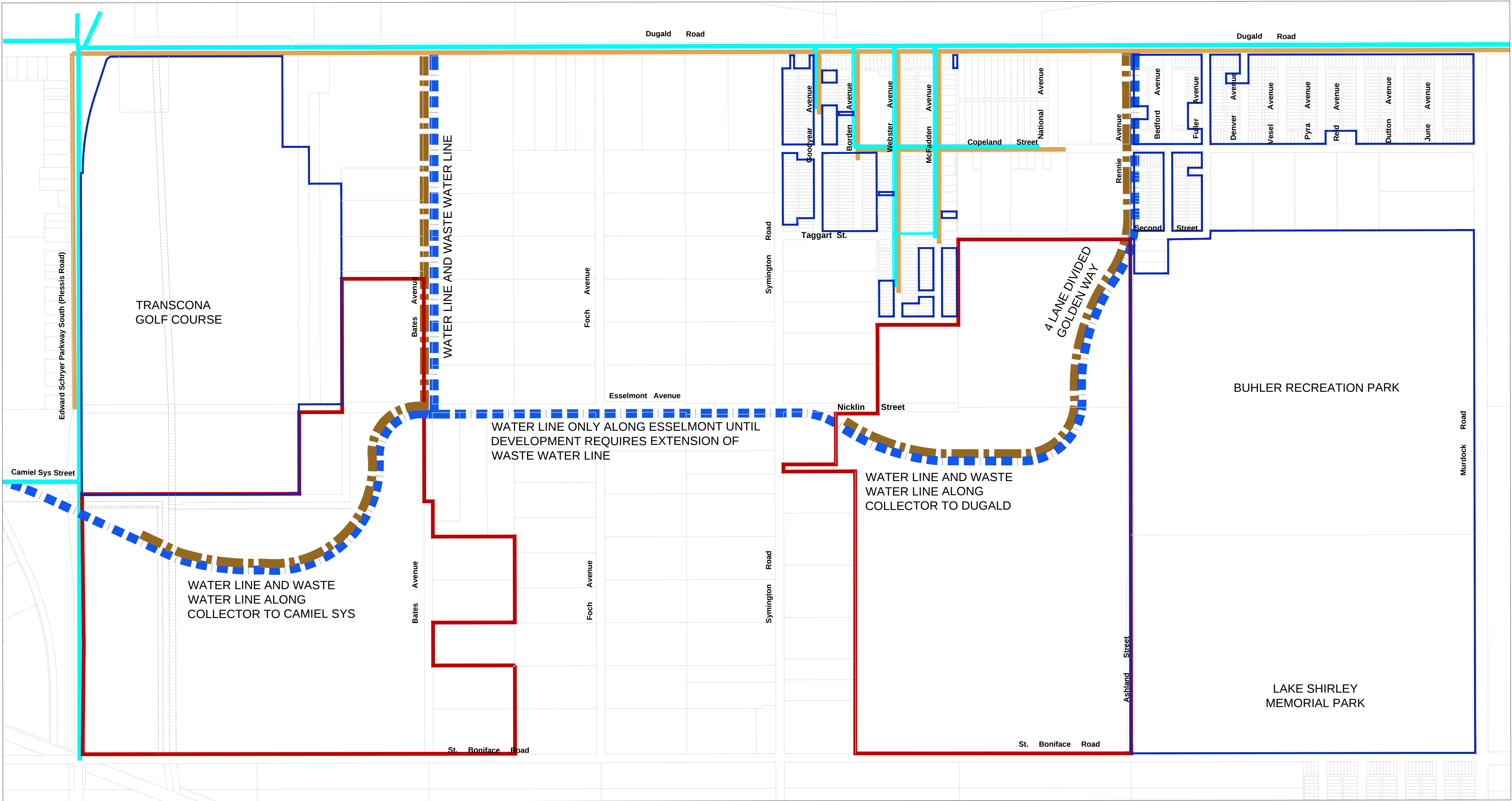
	AREA	REQUIRED		EXISTING PR	PROPOSED PR	TOTAL PR	PERCENT
		POND	8% PR				
West Development Area	±165.0 ac	±8.25 ac	±12.54 ac	0 ac	±21.6 ac	±21.6 ac	13.8%
Acreages Area	±411.4 ac	±20.4 ac	±31.0 ac	0 ac	±156.5 ac	±156.5 ac	40.1%
East Development Area	±178.5 ac	±8.9 ac	±13.6 ac	0 ac	± 15.0 ac	± 15.0 ac	8.8%
Hamlet	±78.1 ac	±3.9 ac	±5.9 ac	±11.9 ac	±12.0 ac	± 23.9 ac	32.2%
East Hamlet	±102.3 ac	±5.1 ac	±7.8 ac	0 ac	± 37.3 ac	± 37.3 ac	38.4%
Transcona Golf Course	±138.7 ac			±138.7 ac		±138.7 ac	100%
Buhler Recreation Park	±157.9 ac			±157.9 ac		±157.9 ac	100%
Lake Shirley Memorial Park	±81.1 ac			±81.1 ac		±81.1 ac	100%
TOTALS FOR SOUTH TRANSCONA	±1313.0 ac	±46.9 ac	±71.3 ac	±389.6 ac	±242.4 ac	±632.0 ac	49.9%

Note: These numbers are estimates based on the concept plan and subject to change. Park planning derives from the priority of protecting natural areas. The City of Winnipeg would have to acquire lands for anything above required PR, subject to City policy.

Note: Pond areas are as required for development and do not include existing ponds in parks and golf course. PR - Public Reserve land for parks.

LAND DRAINAGE & SERVICING

WATER & WASTEWATER SERVICING

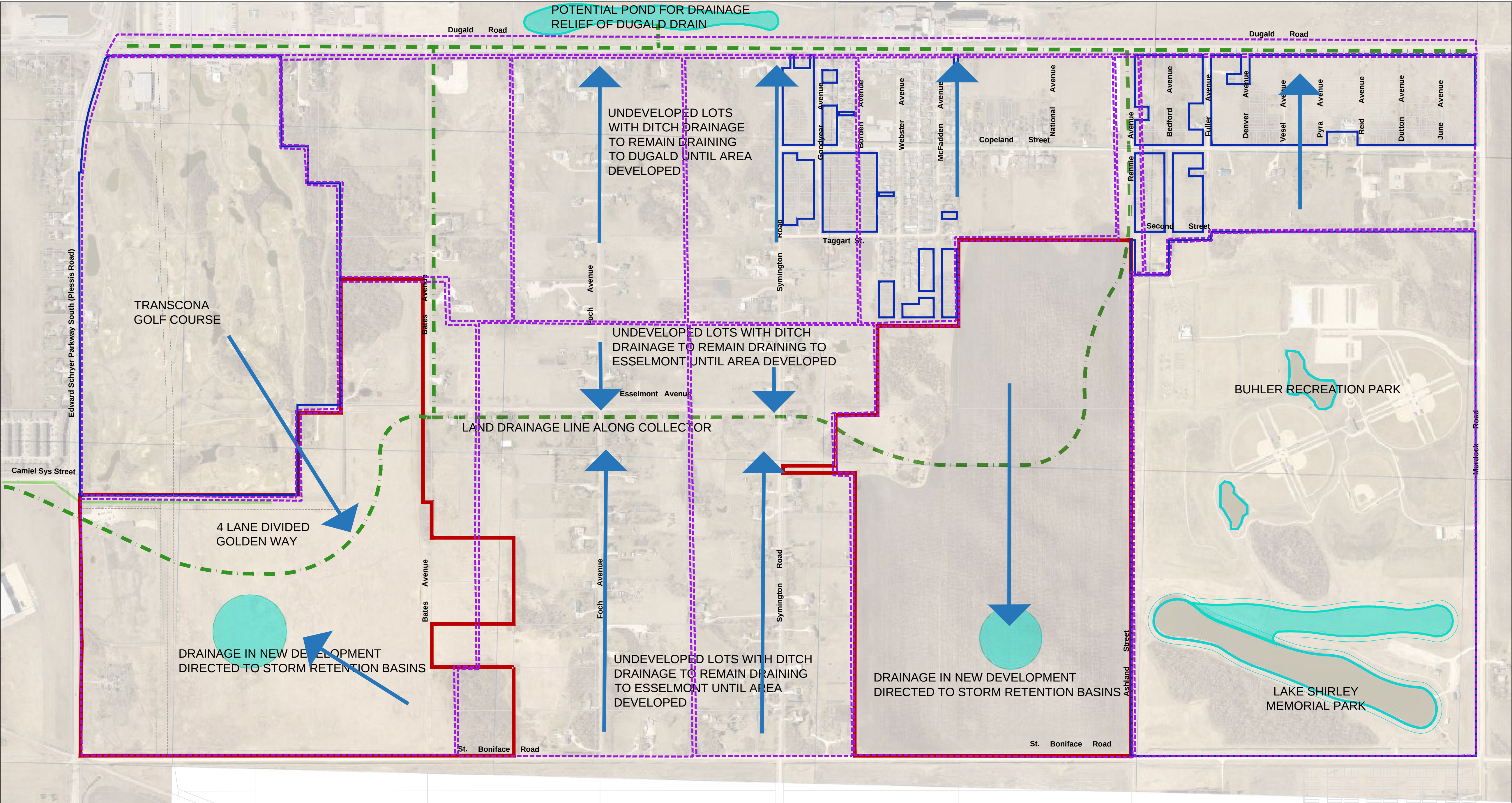


- While the Hamlet Area has traditionally been serviced by municipal sewer and water, the overall area has relied on wells, septic fields and holding tanks.
- The City of Winnipeg expects the area will develop to urban standards over time, which includes municipal water and sewer.
- As services become available, property owners are obligated to connect as per City of Winnipeg policy and Provincial Legislation.

LAND DRAINAGE & SERVICING

LAND DRAINAGE

Note: The Acreages area may require ponds subject to City of Winnipeg land drainage review, if redevelopment occurs.



SOUTH TRANSCONA SECONDARY PLAN
LAND DRAINAGE CONCEPT PLAN

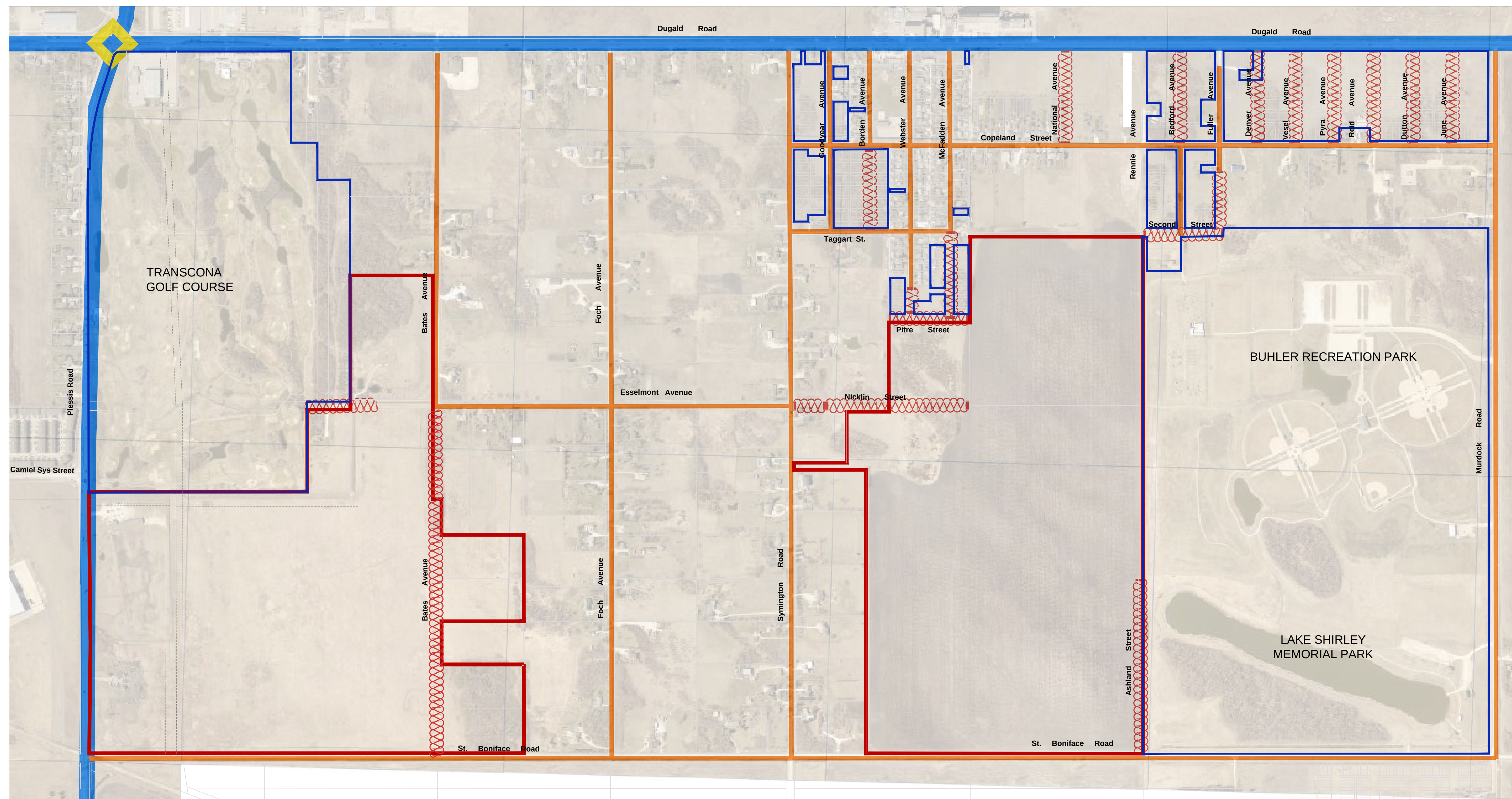
SEPTEMBER 2025
SCALE 1:4000



- PROPOSED SERVICES LEGEND
- LAND DRAINAGE SYSTEM (LDS) PIPE
 - CATCHMENT AREA
 - POND
 - CITY OWNED LAND
 - LANDS OWNED BY MAJOR DEVELOPERS

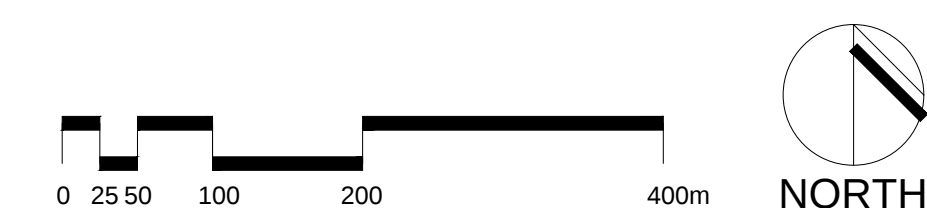
TRANSPORTATION

EXISTING ROAD PLAN



LEGEND

- ARTERIAL / REGIONAL ROAD
- LOCAL ROAD
- UNDEVELOPED OPEN RIGHT OF WAY
- SIGNALIZED INTERSECTION



■ Improvements to an urban standard regional road with design, land assembly, and preparation for tender are targeted to occur in 2033 for the twinning of Plessis Road, followed by a target date of the construction of Dugald Road improvements in 2034/2035.

■ Development will require some front-ending of construction costs by developers through the City approval process as per the Transportation Policy.

TRANSPORTATION

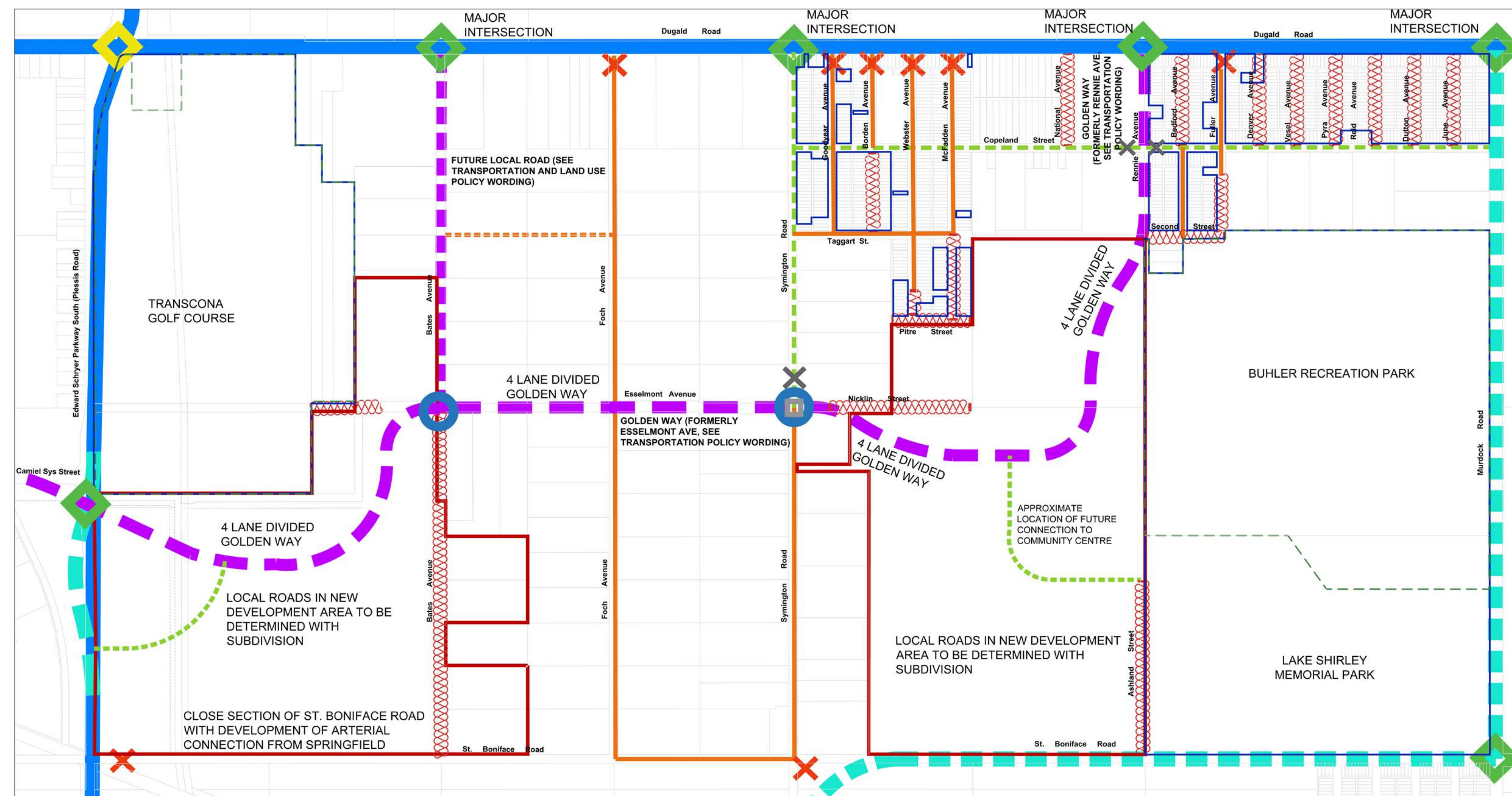
PROPOSED ROAD PLAN

■ The existing arterial roads are Dugald Road and Plessis Road. Future arterials for consideration are St Boniface Road and Murdock Road.

■ The proposed major collector roads include: Esslemont, Bates, and Rennie.

■ The minor collector roads include: Symington Road and Copeland Street and future connections to Golden Way.

■ Eventual closure of local road access to Dugald Road or right-in/right-out only to facilitate more efficient flow along Dugald Road.



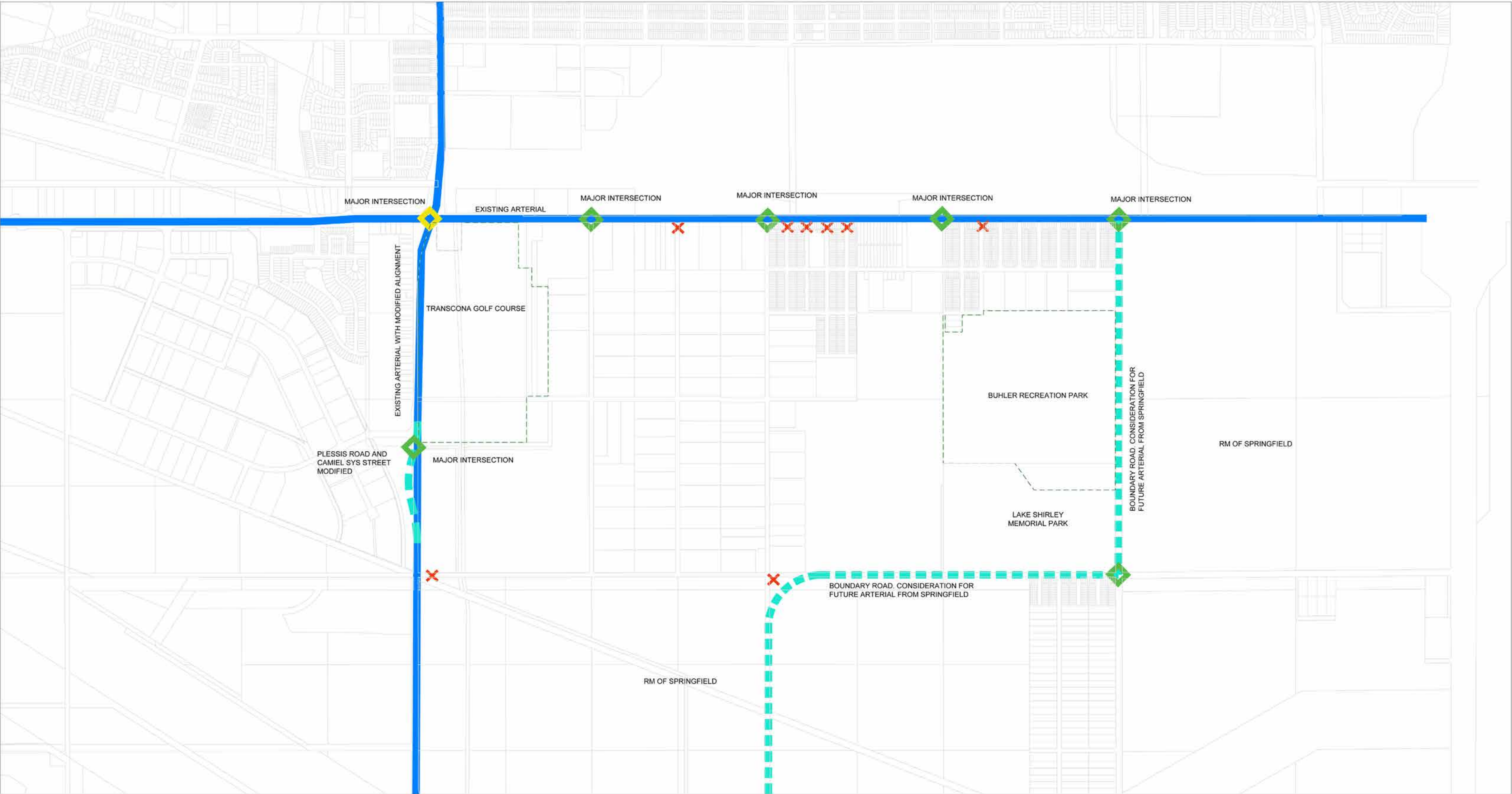
Note: Timing of road upgrades will be phased to minimize impact on existing property owners.

LEGEND

- EXISTING ARTERIAL
 - PROPOSED FUTURE ARTERIAL
 - PROPOSED MAIN STREET 'MAJOR RESIDENTIAL COLLECTOR'
 - - - PROPOSED MAJOR RESIDENTIAL COLLECTOR
 - - - PROPOSED MINOR RESIDENTIAL COLLECTOR
 - EXISTING LOCAL ROAD
 - - - PROPOSED FUTURE LOCAL ROAD
 - ⌘ UNDEVELOPED OPEN RIGHT OF WAY PROPOSED TO BE CLOSED
 - ◊ EXISTING SIGNALIZED INTERSECTION
 - ◊ PROPOSED SIGNALIZED INTERSECTION
 - ◻ PROPOSED PEDESTRIAN CROSSING
 - ⊙ POTENTIAL ROUNDABOUT
 - ✕ EXISTING INTERSECTION PROPOSED TO BE CLOSED
 - ✕ EXISTING INTERSECTION PROPOSED TO BE TEMPORARILY CLOSED
- 0 25 50 100 200 400m NORTH

TRANSPORTATION

ARTERIAL ROAD PLAN



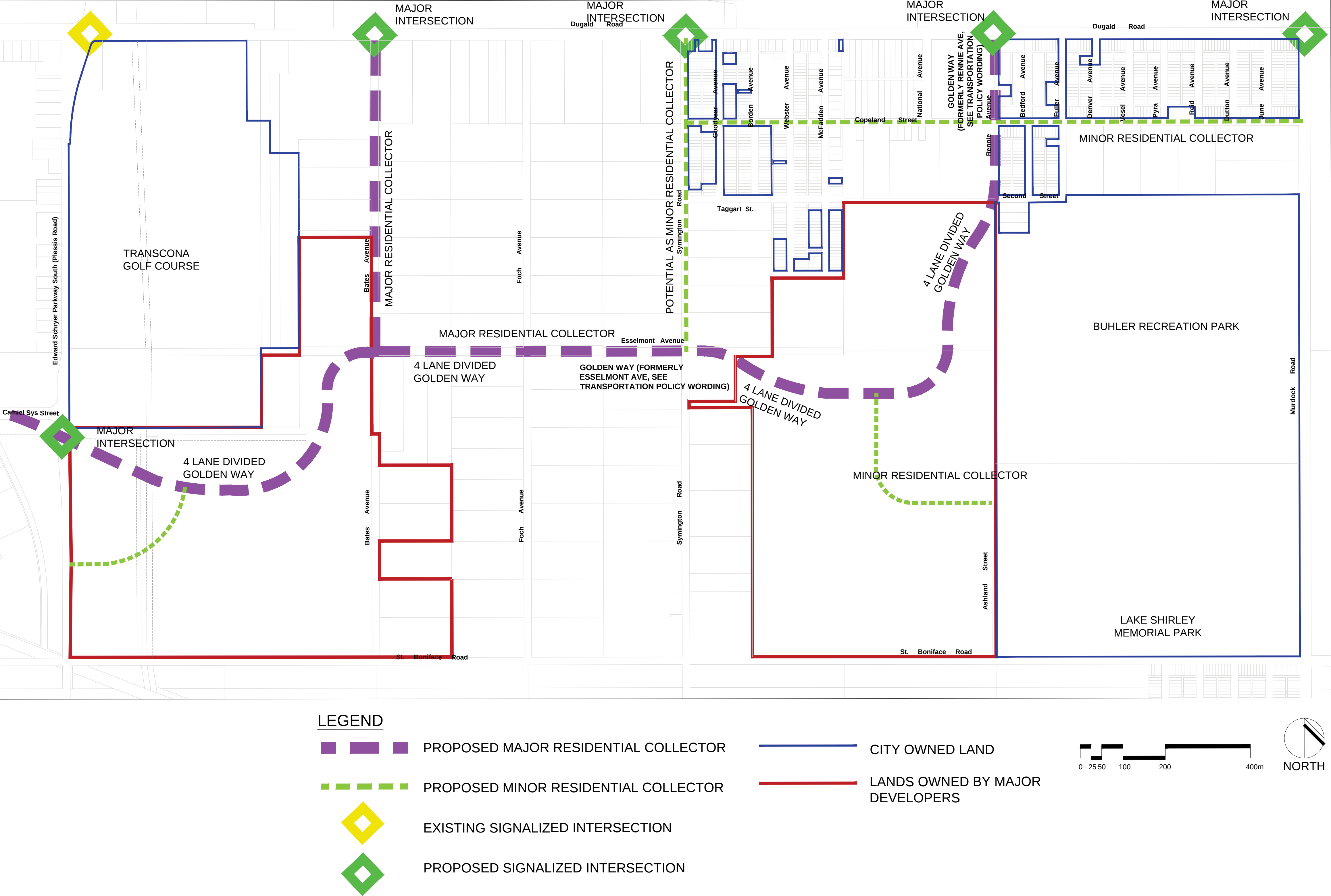
■ Arterial routes were designed so that trucks circumvent the neighbourhood and the RM of Springfield shares in the cost.

■ St. Boniface Road and Symington Road to be disconnected and rerouted, ensuring truck traffic is directed to Murdock Road.

Note: Pending discussion and review between the City of Winnipeg, RM of Springfield, and the Province of Manitoba.

TRANSPORTATION

COLLECTOR ROAD PLAN



- As new neighbourhoods are built out and reach agreed-upon growth milestones, paving of key gravel roads will be required. The first priorities are Esselmont Avenue and Bates Avenue.
- Exact timing is tied to building progress and will be confirmed through City-approved development agreements.
- Secondary roads will be protected from impacts by temporary strategic closures.
- Development will require some front-ending of construction costs by developers through the City approval process as per the Transportation Policy.

TRANSPORTATION

ROAD TYPES

Summary of the City of Winnipeg Streets Classification System Categories:

	Locals	Collectors	Arterials - Minor	Arterials - Major
Average Running Speeds (km/h)	20-50	30-60	40-70	50-80
Typical Right-of-way Width (metres)	18.0	22-35	35	≥ 40.0
Typical Pavement Width (metres)	7.5	10-16	16	≥ 16.0

Note: Inclusion of AT results in slightly wider ROW (~2m).

NEXT STEPS

Approval of the Secondary Plan:

The following is subject to ongoing work by engineers to review Plan recommendations as well as future and ongoing studies. The results of which will be made available at the final community consultation (where available).

1 Final community engagement in November or December, 2025, depending on engineering results.

2 Application Submission to City of Winnipeg

- Project materials (plan, maps, policies) submitted for review.

3 Public Hearing & Community Committee

- Public notice, opportunities for comment, hearing on proposed plan
- Community Committee makes recommendation to SPC.

4 Standing Policy Committee (SPC) & Executive Policy Committee (EPC)

- SPC reviews and recommends to EPC/Council.
- EPC reviews and makes recommendation to Council.

5 City Council Decision

- Council will consider amendments by the upper committees and any recommendations to amend, reject, or approve.
- Council gives readings, may make amendments before final adoption.

6 Opportunity to Appeal Council Decision to Municipal Board

- If the application is approved or denied, objections may trigger referral to Municipal Board under provincial law.

7 By-Law Comes into Effect

- Secondary plan is binding as a by-law after final reading.

DEVELOPMENT STEPS

PENDING SECONDARY PLAN APPROVAL FROM COUNCIL

1. Subdivision and Rezoning
Neighborhood Consultation and Public Open Houses prior to submission of applications.
2. Subdivision & Rezoning Applications and Approval Process including Public Hearing at Community Committee, SPC, EPC and 3 readings at Council
3. Land parcels must be subdivided and rezoned per approved secondary plan policies
4. Preparation of development agreements and registration of subdivision, rezoning and development agreements at Land Titles
5. Development Plan Approvals, Permits & Agreements
6. Construction Start (Earliest: 2028+)
7. Infrastructure and built form phasing begins after approvals

Ongoing Public Involvement: Public can comment or appeal on rezoning, subdivision, and conditional uses under the plan framework.

FUTURE /ONGOING STUDIES

Several technical and partnership studies are underway or planned to guide the long-term development of South Transcona and will be integrated into the Secondary Plan:

1. **Community Facilities** – Feasibility study for a new Community Centre and potential Homeowners Association (HOA).
2. **Parks & Recreation** – Detailed Parks Plan and Business Case to identify future park amenities and funding (including an Area Charge).
3. **Sustainable Energy** – Exploring geo-thermal potential in collaboration with the Province of Manitoba (Efficiency MB study applied).
4. **Partnership Opportunities** – Evaluation of joint venture scenarios with Buhler Recreation Park, Waterski Park, and Transcona Golf Course.
5. **Servicing Capacity** – Comprehensive servicing study to confirm infrastructure requirements for full build-out.
6. **Traffic Study** – Comprehensive traffic study to confirm road requirements for full build out.
7. **Emergency Services Study** – To determine potential emergency services infrastructure that may be needed for full build out.
8. **Noise Study** – To determine potential development constraints and attenuation requirements related to rail noise.

SHARE YOUR FEEDBACK

Your feedback is a crucial step in this process. Please take 10 minutes to fill out a survey (paper or digital) and share your thoughts.

